











Uptown/Seattle Center/Belltown Republican/Key Arena

Station-Specific Background Issues

Natural Features



Critically and Environmentally Sensitive Areas

	Steep Slope (>40%)		Flood Prone
	Known Slide Area		Riparian Corridor
	Potential Slide Area		Landfill
	Wetlands		Liquefaction Zone
	Wildlife Area		10' Contour

Source: City of Seattle Design, Construction and Land Use, March, 2000.

- In the area near the station, First Avenue has a moderate slope to the south; Harrison and Republican slope downward to the west toward Elliott Bay.
- The steep slope of Queen Anne Hill is north of Roy Street. The grade of Queen Anne Hill is significant enough that some streets are closed to vehicles, with stairs for pedestrians.

Existing Character



- This station is near the middle of the Uptown urban center, at the northwest corner of Seattle Center.
- Outside of Seattle Center are a mix of new mixed-use and office structures up to six stories; older retail or light industrial buildings with one or two stories; three- to four-story apartment buildings

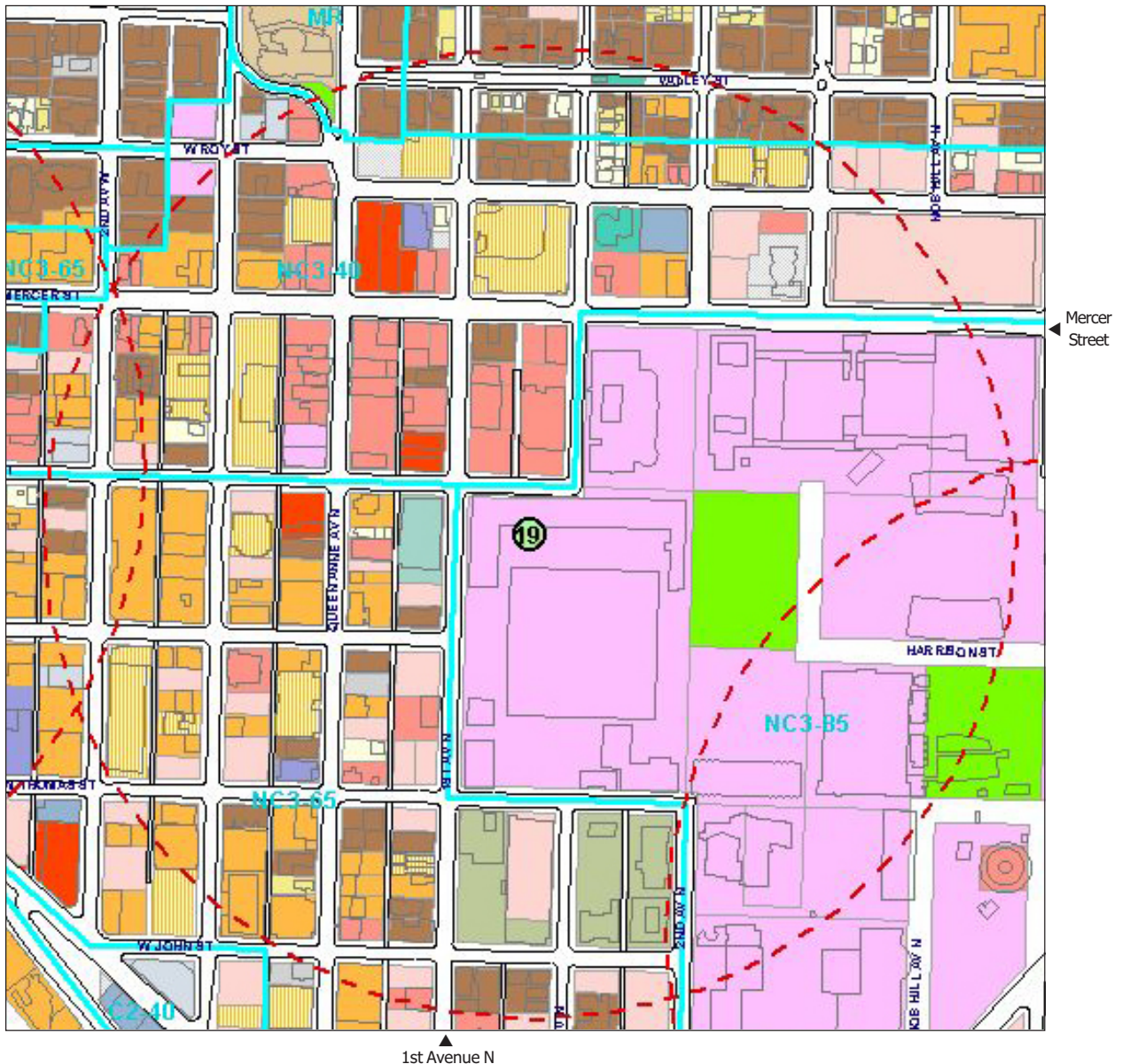
- dating to the 1920s; and surface parking lots.
- Retail storefronts are concentrated along First and Queen Anne Avenues, and Mercer Street.
- Three blocks north of the station begins a dense multifamily area, which stretches up the steep hillside of Queen Anne Hill to the north.

|||| Spatial Patterns



- The station area includes several very large building footprints of performance or sports venues.
- One of Seattle's largest urban open spaces, Seattle Center's International Fountain area is within the station area.
- The Northwest Rooms form a wall along much of Republican Street and part of First Avenue.
- Along First Avenue, the fabric of the streetfront is incomplete, with a mix of building frontage and parking areas. The entrance to Key Arena is also a major opening along the street.
- The station area includes mid-size to large scale office and residential buildings west of First Avenue.
- Uptown's retail frontage is fairly complete.

Land Use Notes



- The Uptown retail core contains a variety of restaurants, stores and services along Mercer,

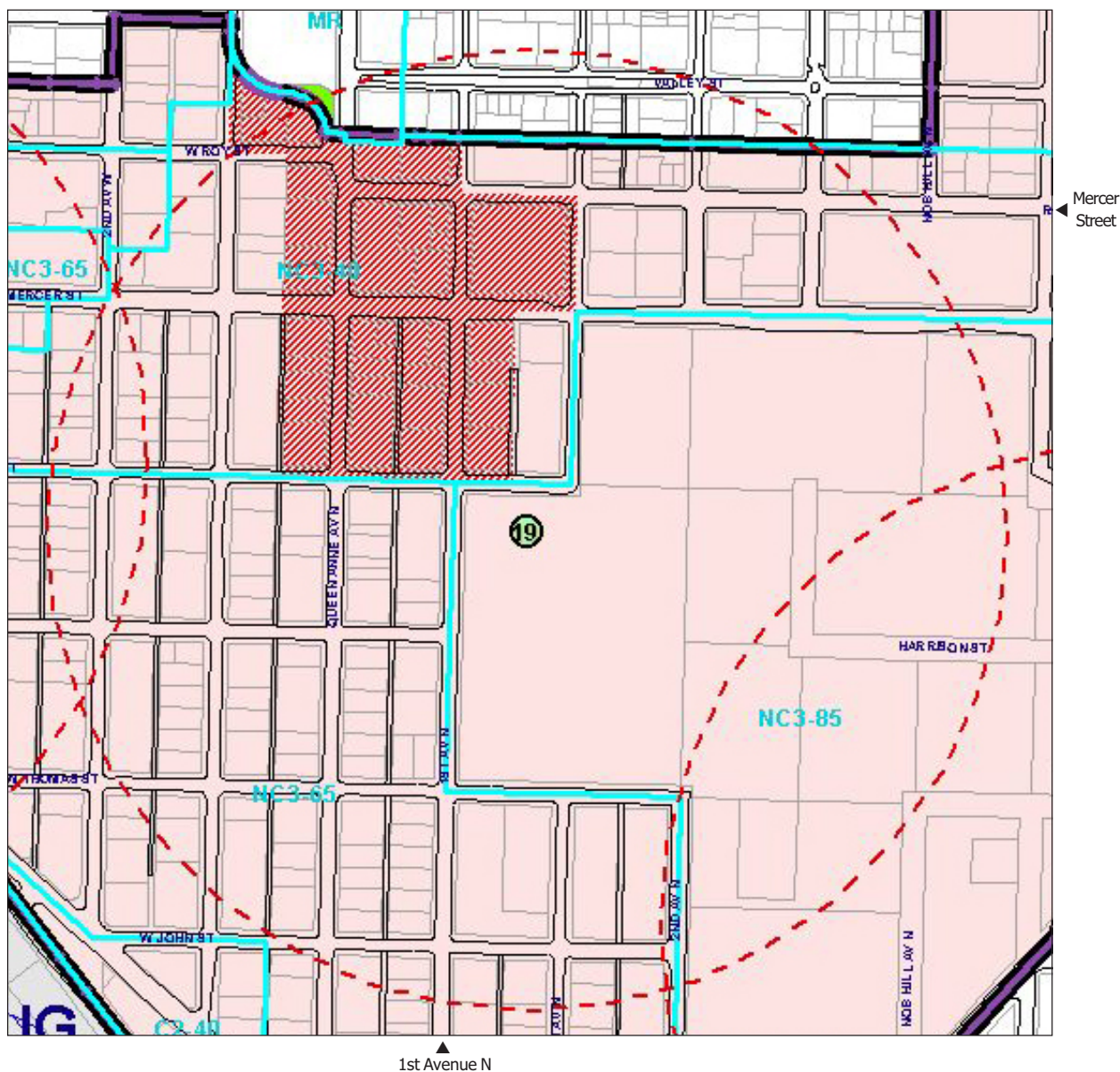
Existing Land Use and Zoning			
Single Family	Entertainment	Public Facilities	
Duplex/Triplex	Mixed Use	Schools	
Other Housing	Parking	Zoning	
Multi-Family	Industrial	Open Space	
Office	Warehouse	Vacant	
Retail/Service	Transp/Util/Comm	Water Body	
Hotel/Motel	Institutions	Unknown/No Data	

Source: Land Use, King County Assessor, January 2003; Zoning, City of Seattle Design, Construction and Land Use, March 2003.























First Avenue West and Queen Anne Avenue West. Multi-family housing is distributed in the station area, with an increasing amount of commercial office in the west portion of the station area.

- Some of Seattle's most important performance venues lie within the station area, including Key Arena, the Repertory Theater, the Intiman Theater, the Seattle Children's Theater, the Pacific Science Center and Marion O. McCaw Hall.
- Seattle Center is a major regional attraction, and an important open space for major gatherings and events, as well as less intensive daily use.

Zoning Notes



Zoning, Overlays and Village Designations

 Zoning	 Urban Center/Village	 Conservancy Navigation (CN)
 Historic District	 Hub/Residential Urban Village	 Conservancy Preservation (CP)
 Pedestrian Overlay	 Manufacturing & Industrial Center	 Conservancy Recreation (CR)
 P1	 Urban Center	 Conservancy Waterway (CW)
 P2	 Shoreline Overlay	 Urban General (UG)
 Major Institution	 Conservancy Maritime (CM)	 Urban Harbortown (UH)
		 Urban Industrial (UI)
		 Urban Maritime (UM)
		 Urban Recreation (UR)
		 Urban Stable (US)

Source: City of Seattle Design, Construction and Land Use, March 2003.

- *The Seattle Center West proposed station is surrounded both by the Seattle Center grounds to the east and primarily Neighborhood Commercial 3 (NC3) zoning with height limits ranging from 45' to 65' to the west, allowing the broadest range of commercial uses, goods and services in a pedestrian-oriented environment. Mixed uses (commercial and residential) are encouraged. Housing*

development in mixed use buildings is not limited as to density.

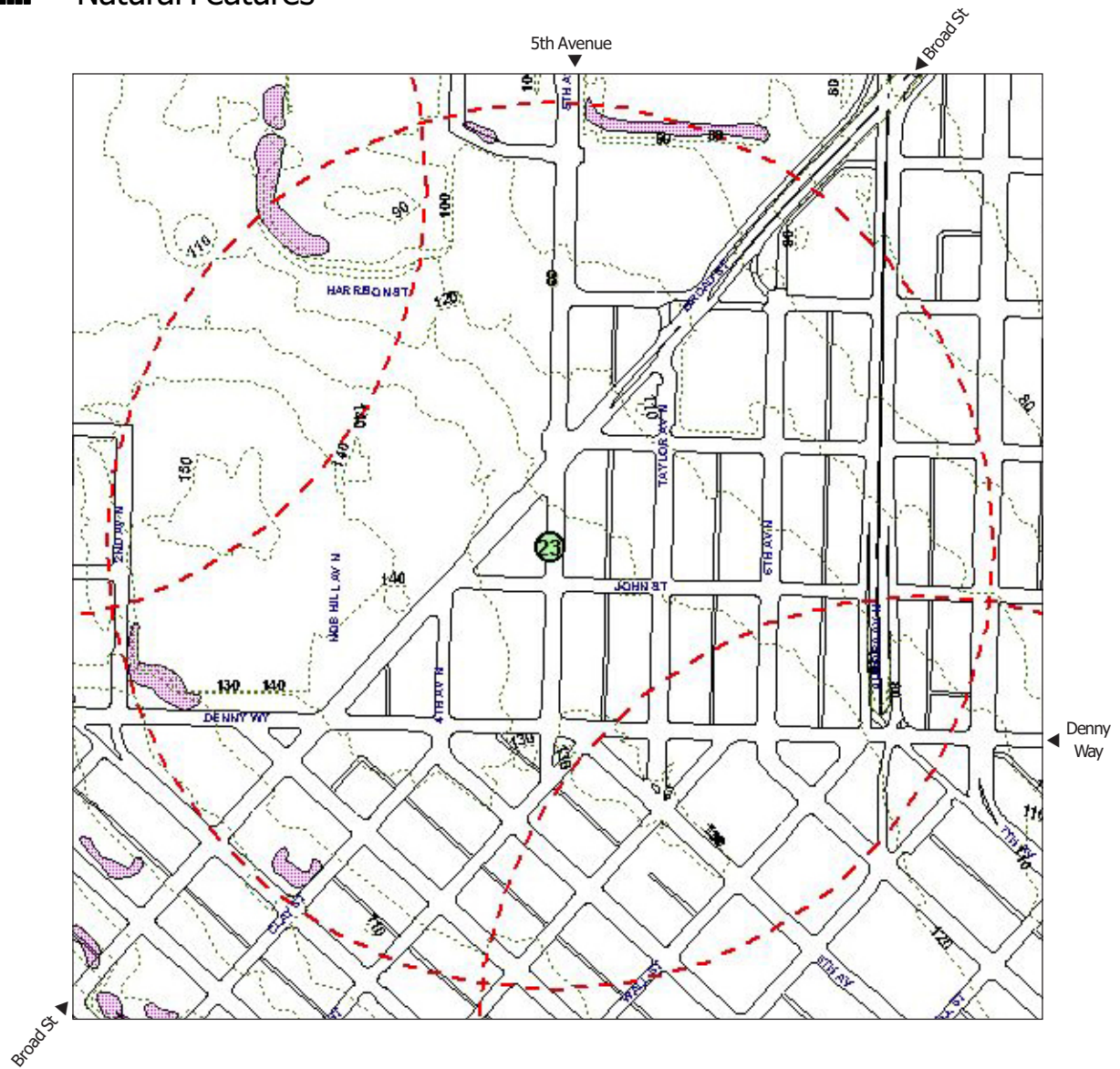
- *The area generally bounded by West Republican Street, West Roy Street, encompassing the Queen Anne Avenue North and First Avenue North corridors represents the pedestrian core of the*

Uptown neighborhood. Within this pedestrian designated area, immediately to the northwest of the proposed station, retail sales and services are required along the street front, parking substantially waived for businesses and is otherwise restricted to in, under or behind a building.

Uptown/Seattle Center/Belltown 5th & John

Station-Specific Background Issues

Natural Features



Critically and Environmentally Sensitive Areas

	Steep Slope (>40%)		Flood Prone
	Known Slide Area		Riparian Corridor
	Potential Slide Area		Landfill
	Wetlands		Liquefaction Zone
	Wildlife Area		10' Contour

Source: City of Seattle Design, Construction and Land Use, March, 2000.

- The station area is relatively flat, and easily walkable. At the northeast edge of the station area, the land slopes down toward the south end of Lake Union, and at the southwest edge of the station area, the topography falls toward Elliott Bay.

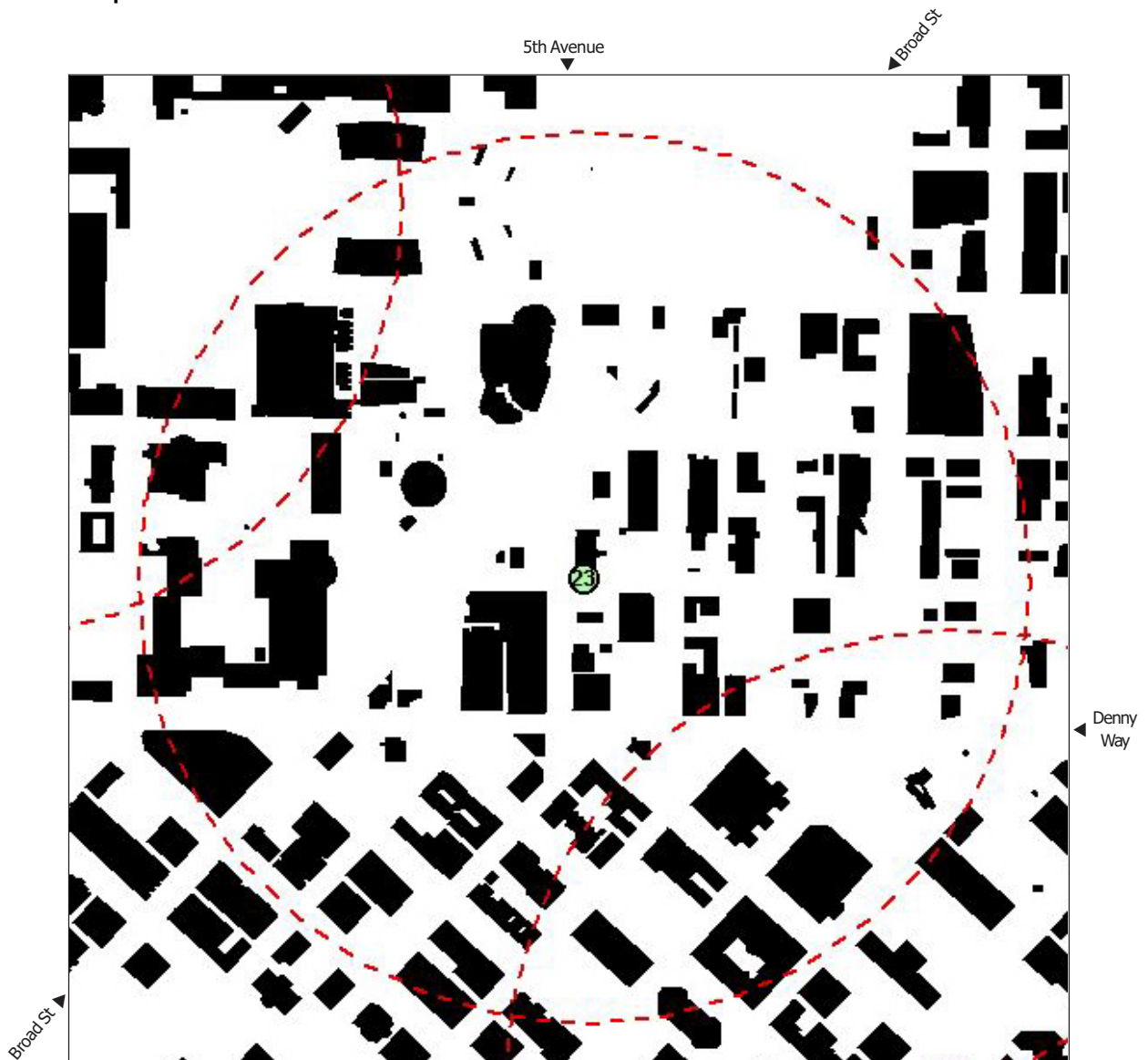
Existing Character



- This station is located just outside of four urban villages: Uptown Urban Center, Belltown, Denny Triangle and South Lake Union.
- Seattle Center is to the northwest of the station.
- Auto-oriented uses lace the major arterials (Aurora, Denny, Broad and Fifth) which cross through the area.

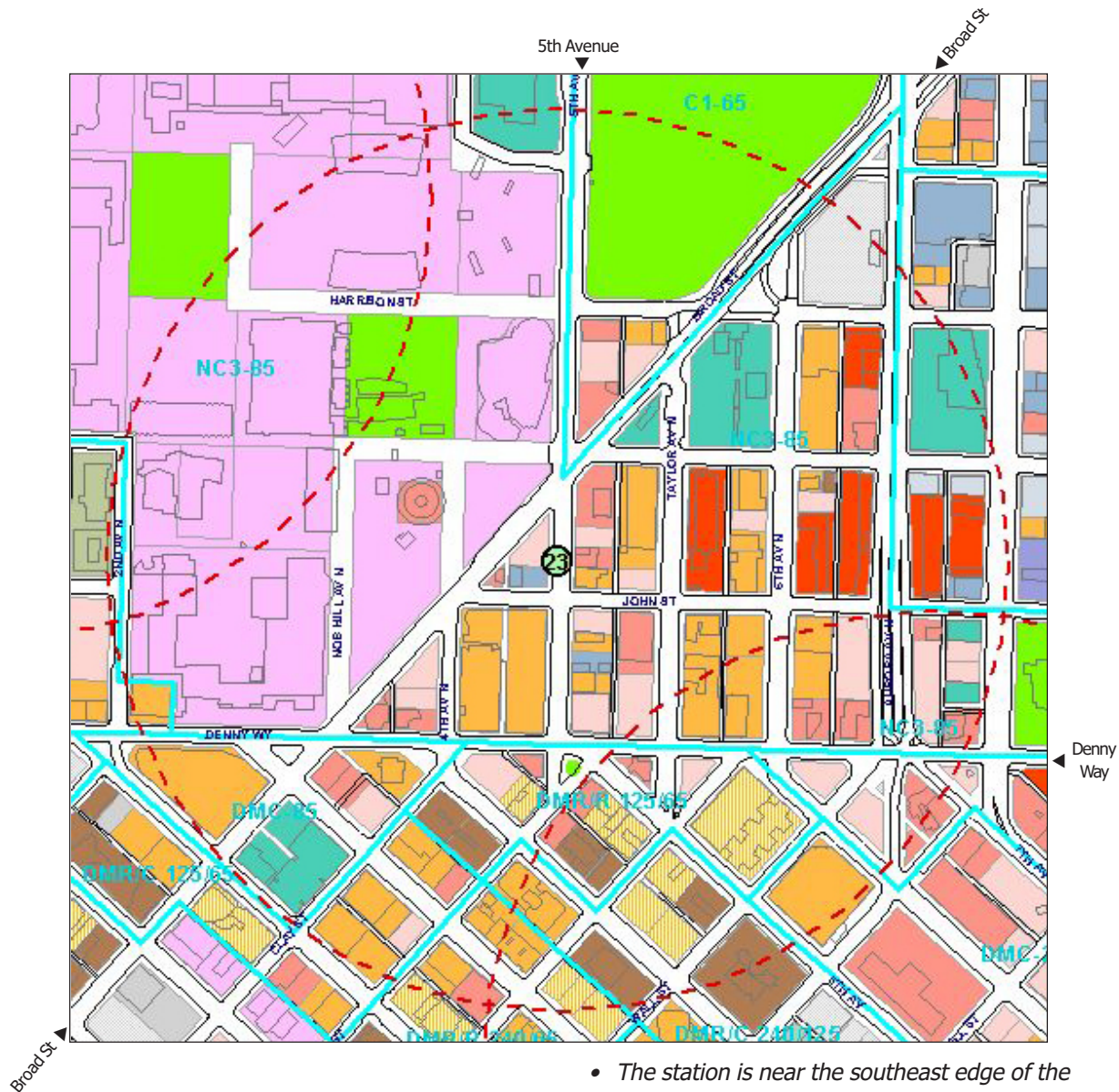
- The character of the area is also molded by the denser neighborhoods that surround it: there are a number of denser office buildings and residential towers scattered throughout the area.
- Broad Street Green is an open, landscaped area along Broad Street, recently created as an inviting edge to Seattle Center.

■■■■ Spatial Patterns



- Building edges do not typically define the street edges in the station area. The edge of Seattle Center has been opened up along Broad Street, and buildings along the Center's edge tend to be sculptural.
- Broad Street runs at a diagonal to the street grid, creating a series of triangular parcels. Denny forms the boundary between the grid system aligned north-south, and the shifted grid of Belltown that is oriented toward the geometry of Elliott Bay. This grid shift creates triangular parcels on the south side of Denny.
- East of Broad Street, development and parking are interspersed. A sunken portion of Aurora Avenue (SR-99) lies on the east edge of the station area. It is a significant barrier, especially for pedestrians and bicyclists.

Land Use Notes



Existing Land Use and Zoning

Single Family	Entertainment	Public Facilities
Duplex/Triplex	Mixed Use	Schools
Other Housing	Parking	Zoning
Multi-Family	Industrial	Open Space
Office	Warehouse	Vacant
Retail/Service	Transit/Bus Comm	Water Body
Hotel/Motel	Institutions	Unknown/No Data

Source: Land Use, King County Assessor, January 2003; Zoning, City of Seattle Design, Construction and Land Use, March 2003.

- The station is near the southeast edge of the Seattle Center, in proximity of the Space Needle, the Experience Music Project, the open space of the Broad Street Green, the IMAX Theater and the Pacific Northwest Science Center.
- Large surface parking areas on the north edge of the station area serve Seattle Center.
- East of the station, land is not typically developed to allowable capacity.
- South of Denny Way in the Belltown neighborhood, mixed use predominates.
- The park at Fifth Avenue and Denny Way is an urban oasis, with a historic statue of Chief Seattle.

Zoning Notes



Zoning, Overlays and Village Designations		
Zoning	Urban Center/Village	Conservancy Navigation (CN)
Historic District	Hub/Residential Urban Village	Conservancy Preservation (CP)
Pedestrian Overlay P1	Manufacturing & Industrial Center	Conservancy Recreation (CR)
Pedestrian Overlay P2	Urban Center	Conservancy Waterway (CW)
Major Institution	Shoreline Overlay	Urban General (UG)
	Conservancy Maritime (CM)	Urban Harborfront (UH)
		Urban Industrial (UI)
		Urban Maritime (UM)
		Urban Recreation (UR)
		Urban Stable (US)

Source: City of Seattle Design, Construction and Land Use, March 2003.

- Generally, this station area encompasses the southwest quadrant of the Seattle Center grounds. Surrounding the proposed station outside of the Seattle Center boundary, the area is zoned Neighborhood Commercial 3 (NC3) with a 85' height limit, allowing the broadest range of commercial uses, goods and services in a pedestrian-oriented environment.

- *Mixed uses (commercial and residential) are encouraged. Housing development in mixed use buildings is not limited as to density.*
- *To the south of the proposed station across Denny Way, the station area enters the downtown urban village and zoning. Zones within this edge of downtown are Downtown Mixed Commercial (DMC) and Downtown Mixed Residential (DMR). Height limits range from 85' to 125'.*
- *DMR zoning encourages primarily residential development. However, other compatible uses are allowed to the extent that they reinforce and do not detract from the primarily residential function of the area. An additional /R suffix is added here to indicate that this area is predominantly residential in character or contains large amounts of underutilized land, and is not characterized by more than modest scaled non-residential uses.*
- *DMC zoning is generally a transition commercial area to the downtown office core and retail core area. It is characterized by a diversity of uses, including office and other commercial use. Housing and other activities that do not contribute significantly to peak hour traffic demand are also permitted and encouraged. A multiplicity of height limits may accompany a DMC zone.*
- *A very small area to the east of the proposed station area falls within the South Lake Union Urban Village and is currently zoned Commercial 1 (C1) with an 85' height limit. C1 zones are auto-oriented areas where retail uses with substantial parking supply are historically present or able to be accommodated easily. While residential uses are permitted they are not preferred.*













Uptown/Seattle Center/Belltown 5th & Bell

Station-Specific Background Issues

Natural Features



Critically and Environmentally Sensitive Areas

 Steep Slope (>40%)	 Flood Prone
 Known Slide Area	 Riparian Corridor
 Potential Slide Area	 Landfill
 Wetlands	 Liquefaction Zone
 Wildlife Area	 10' Contour

Source: City of Seattle Design, Construction and Land Use, March, 2000.

- The station area is quite flat, and easily walkable. Southwest of the station area, the land falls toward Elliott Bay.

Existing Character

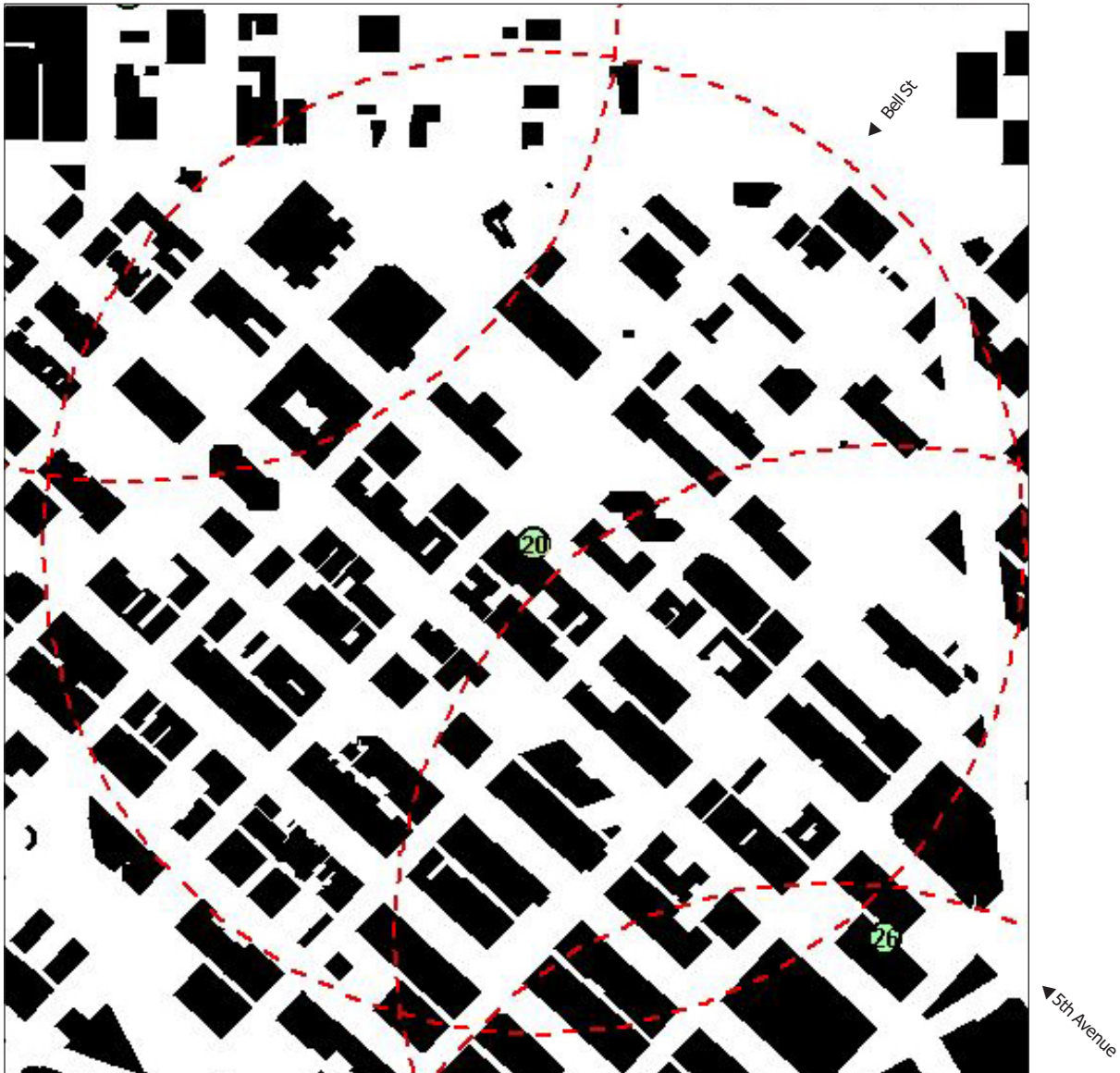


- This station is located on the border between Belltown and the Denny Triangle.
- In the Denny Triangle, there are few existing buildings to define the character. The area contains a number of surface parking lots, motels, one-story retail structures, and a few office towers and older residential buildings, but there is little to

walk to and little pedestrian environment.

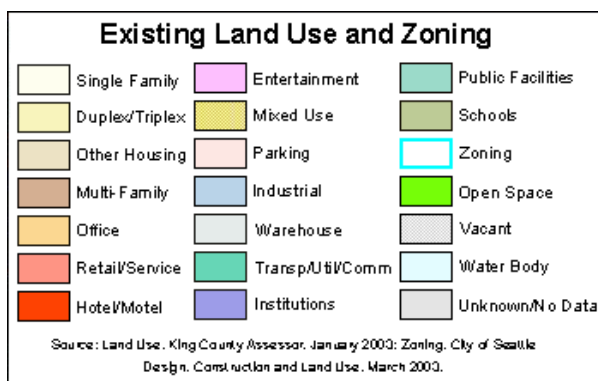
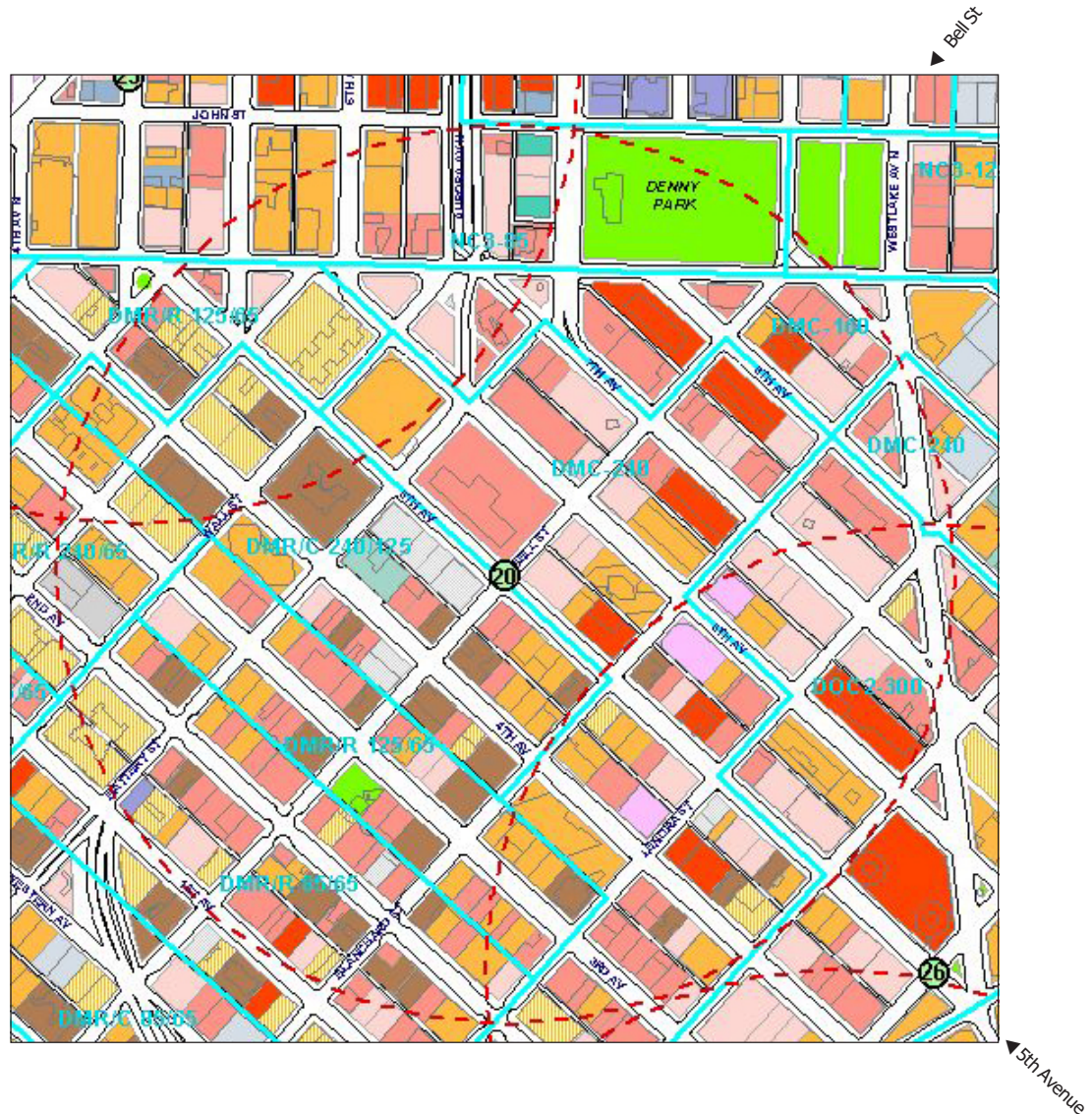
- In Belltown, there are fewer surface parking lots, more residential and mixed-use towers, and a few office towers.
- The retail street frontage is more clearly defined in Belltown than in the Denny Triangle, particularly along First and Second Avenues.

|||| Spatial Patterns



- West of Fifth Avenue, the urban fabric has been filled out with new mixed-use development. The blocks east of Fifth Avenue are more sparsely developed, with a number of large surface parking lots.
- Almost all of the station area has the street grid that parallels Elliott Bay, even though the water itself is not visible. At the edges of the station area, Denny Way and Westlake define other street geometries, with resultant triangular blocks.






Land Use Notes



- Development west of Fifth Avenue includes an urban mix of residential, retail, restaurants and commercial offices.
- East of Fifth Avenue, there is almost no residential use. A grouping of hotels lies to the northeast, and a mix of parking lots and varied commercial uses.
- Denny Park is partially within the station area, to the northeast of the station.

Zoning Notes



Zoning, Overlays and Village Designations		
	Urban Center/Village	Conservancy Navigation (CN)
	Hub/Residential Urban Village	Conservancy Preservation (CP)
	Manufacturing & Industrial Center	Conservancy Recreation (CR)
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		Urban Industrial (UI)
		Urban Maritime (UM)
		Urban Recreation (UR)
		Urban Stable (US)

Source: City of Seattle Design, Construction and Land Use, March 2003.

- This proposed station is centrally located within the Belltown area of downtown.
- Zones within this part of downtown are Downtown Mixed Commercial (DMC) and Downtown Mixed Residential (DMR). Height limits range from 85' to 240'. Generally, housing, a preferred use within the DMR zone, is allowed to attain a higher height than commercial development.

- *DMR zoning is primarily residential in character. However, other compatible uses are allowed to the extent that they reinforce and do not detract from the primarily residential function of the area. An additional /R suffix is added here to indicate that this area is predominantly residential in character or contains large amounts of underutilized land and is not characterized by more than modest scaled non-residential uses. Where accompanied by a /C suffix, commercial development is generally allowed at greater density and scale. In /C suffix areas, larger commercial uses are present and likely to remain.*
- *DMC zoning is generally a transition commercial area to the downtown office core and retail core area. It is characterized by a diversity of uses, including office and other commercial use. Housing and other activities that do not contribute*

significantly to peak hour traffic demand are also permitted, and encouraged. A multiplicity of height limits may accompany a DMC zone.

- *A small portion of the station area is zoned Downtown Office Core 2 (DOC2) with a 300' height limit. The density of use is limited by a Floor Area Ratio (FAR) which relates the total area, in square feet, of a building site or parcel of land to the total area, in square feet, within the building.*
- *The DOC2 zone is generally adjacent to the primary office core downtown. In this particular location, it serves as a transition to the lower density mixed use area described above that constitutes the majority of this station area. Although intended primarily for office uses, other uses are encouraged to add diversity, particularly after the normal working day.*